

ARTICLE IV

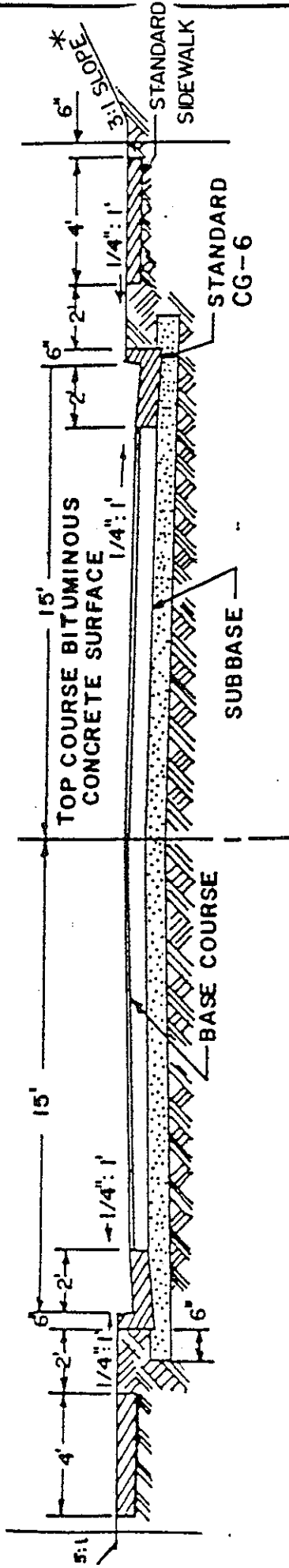
STREETS

SECTION 404.00 - ROAD DESIGN STANDARDS AND SPECIFICATIONS

<u>Detail</u>	<u>Plate</u>
Typical Section - Private Townhouse, Condominium, or Apartment Street with no Residential Frontage	IV-A
Intersection Landing Requirements for Grades between 2% and 7%	IV-B
Intersection Landing Requirements for Grades between 7% and 9%	IV-C
Private Driveway Entrance - Ditch Section	IV-D
Typical Street Sections	IV-E
Standard Entrance/Pipestem Lots/Curb & Gutter Section	IV-F
Standard Driveway Entrance DE-1	IV-G

ϕ PVMT.

PLATE IV-A



1. No Residential Frontage Permitted.

2. No Parking Permitted.

3. A Barrier Or Barricade Must Be Installed Between Adjoining Parking Bays And The Dedicated Right of way To Prevent Parked Vehicles From Overhanging The Right-of-way.

4. Distinct Cul-de-sacs Must Be Constructed Where Streets End And Parking Bays Begin.

★ 5. 2:1 Slopes Will Be Allowed Where Special Stabilization Is Provided In Accordance With The Erosion Control Ordinance.

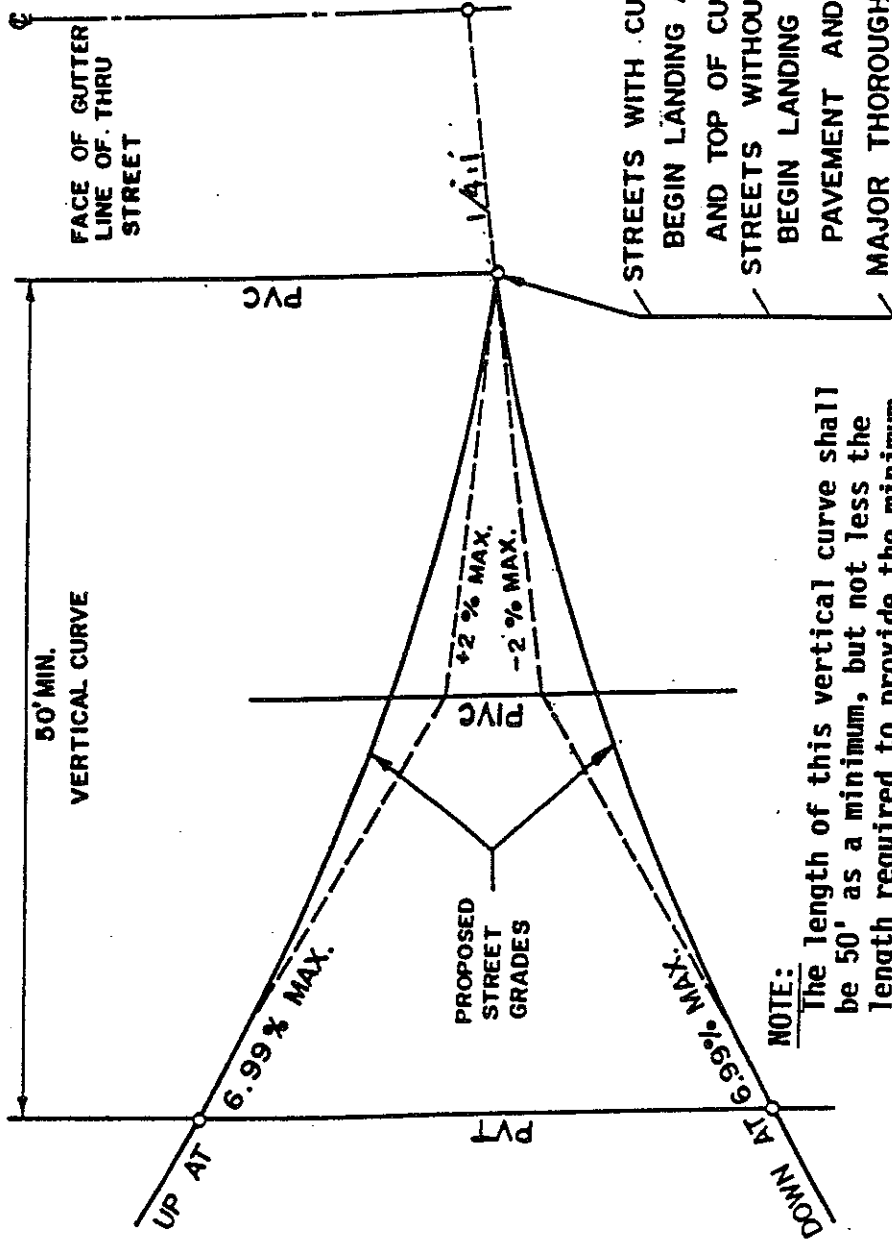
6. Standard Landing Required For Intersections With Public Streets.

7. Ingress/Egress Easements To Be Dedicated To The City Of Manassas Park.

8. Maximum Of 700 Vehicles Per Day For Private Streets.

9. Typical Section - Street Category II (See Plate IV-E).

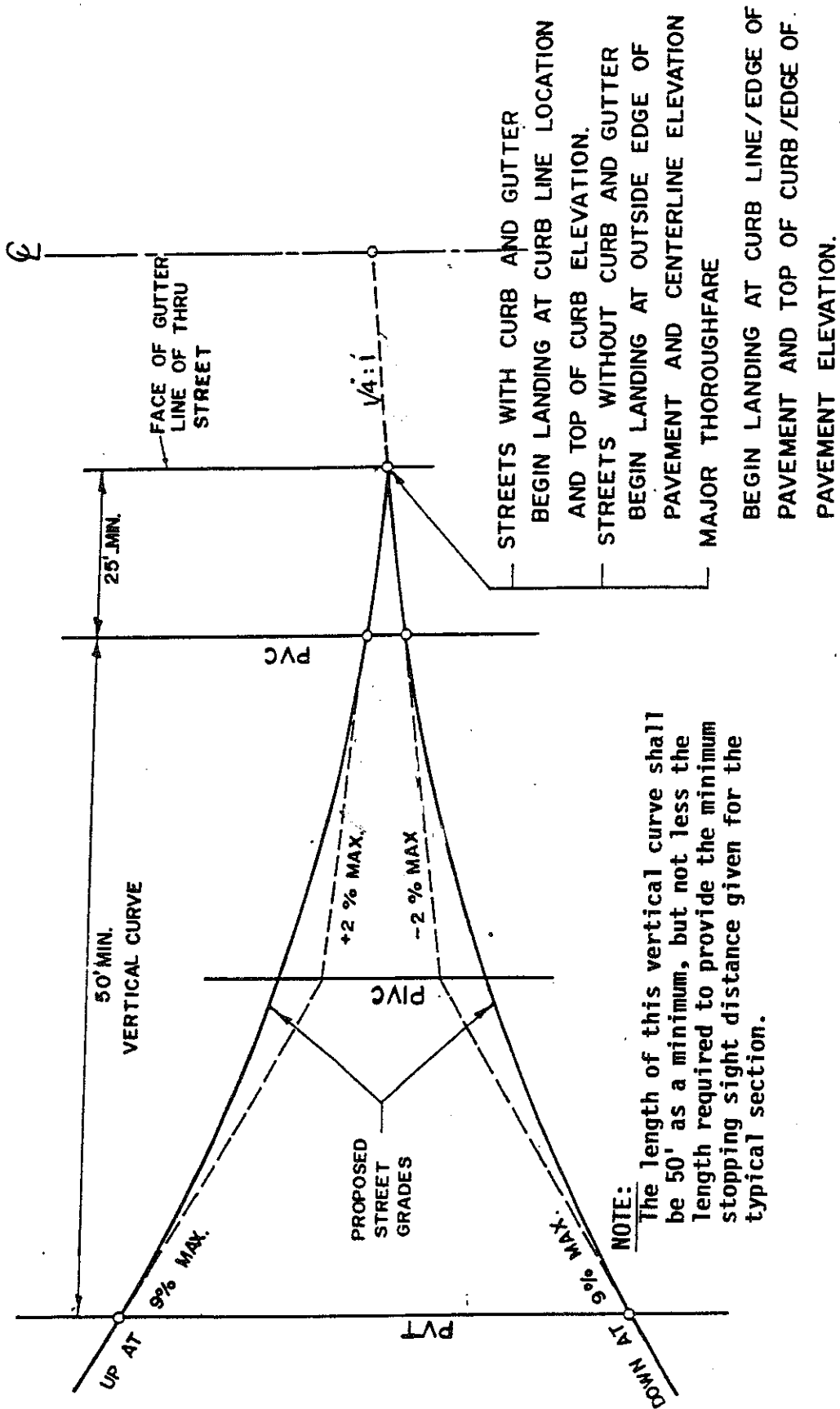
TYPICAL SECTION - TOWNHOUSE, CONDOMINIUM, OR APARTMENT STREET WITH NO RESIDENTIAL FRONTAGE



STREETS WITH CURB AND GUTTER
 BEGIN LANDING AT CURB LINE LOCATION
 AND TOP OF CURB ELEVATION.
 STREETS WITHOUT CURB AND GUTTER
 BEGIN LANDING AT OUTSIDE EDGE OF
 PAVEMENT AND CENTERLINE ELEVATION
 MAJOR THOROUGHFARE
 BEGIN LANDING AT CURB LINE/EDGE OF
 PAVEMENT AND TOP OF CURB/EDGE OF
 PAVEMENT ELEVATION.

NOTE:
 The length of this vertical curve shall
 be 50' as a minimum, but not less the
 length required to provide the minimum
 stopping sight distance given for the
 typical section.

INTERSECTION LANDING REQUIREMENTS FOR GRADES
BETWEEN 28 AND 78

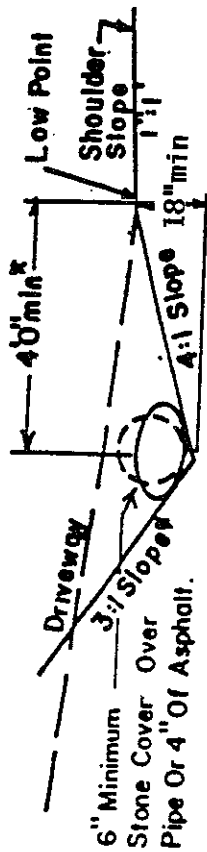


INTERSECTION LANDING REQUIREMENTS FOR GRADES

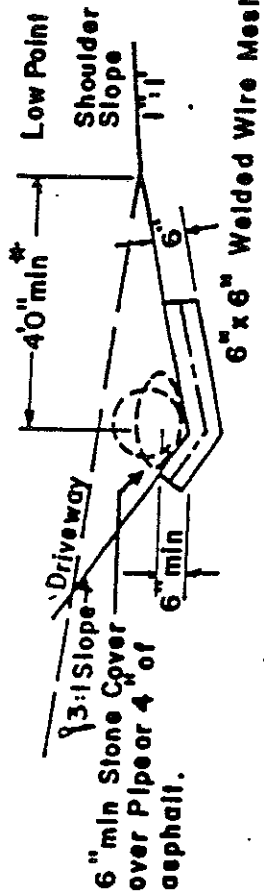
BETWEEN 7% AND 9%

DRIVEWAY CULVERT PIPE INSTALLATION

PLATE IV-D



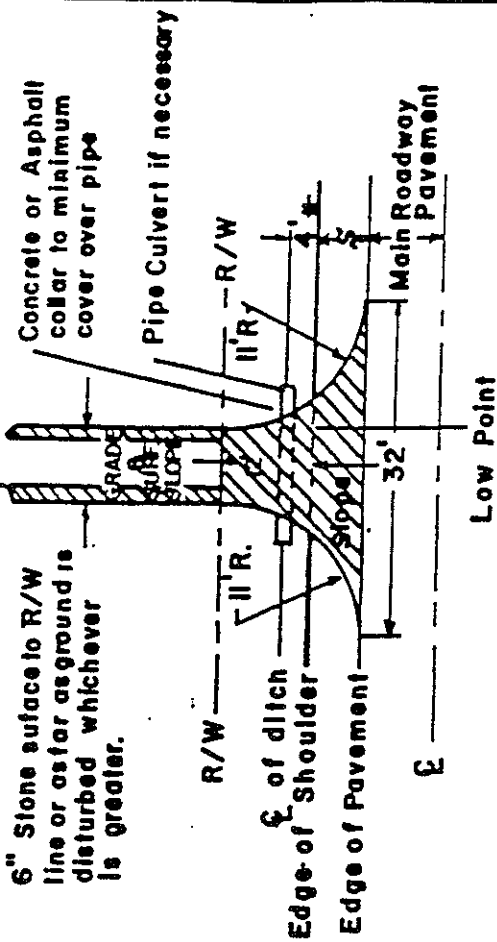
WITH UNPAVED ROADSIDE DITCH



WITH PAVED ROADSIDE DITCH

a paved ditch is required where soil conditions and runoff velocities will cause erosion.

* 2:1 Slopes will be allowed where special stabilization is provided in accordance with erosion and sedimentation control ordinance.



All construction and materials shall conform, where applicable, to the current Virginia Department of Transportation Road and Bridge Specifications.
Concrete pipe or corrugated metal pipe may be used. Indicate type and size on plans.
Driveways shall be surfaced from edge of pavement to property line with the same type of surfacing as used on street.
All driveway grades shall start back of the shoulder line.
In cut sections, sides of driveway shall be graded to a maximum 3:1 slope.
Lengths of culverts if not shown on plans shall be a minimum of 20'.

* Ditch line may be moved back to provide required cover. The transition of the ditch line shall be smooth with a minimum length of 10 feet.
Driveway Clearances.

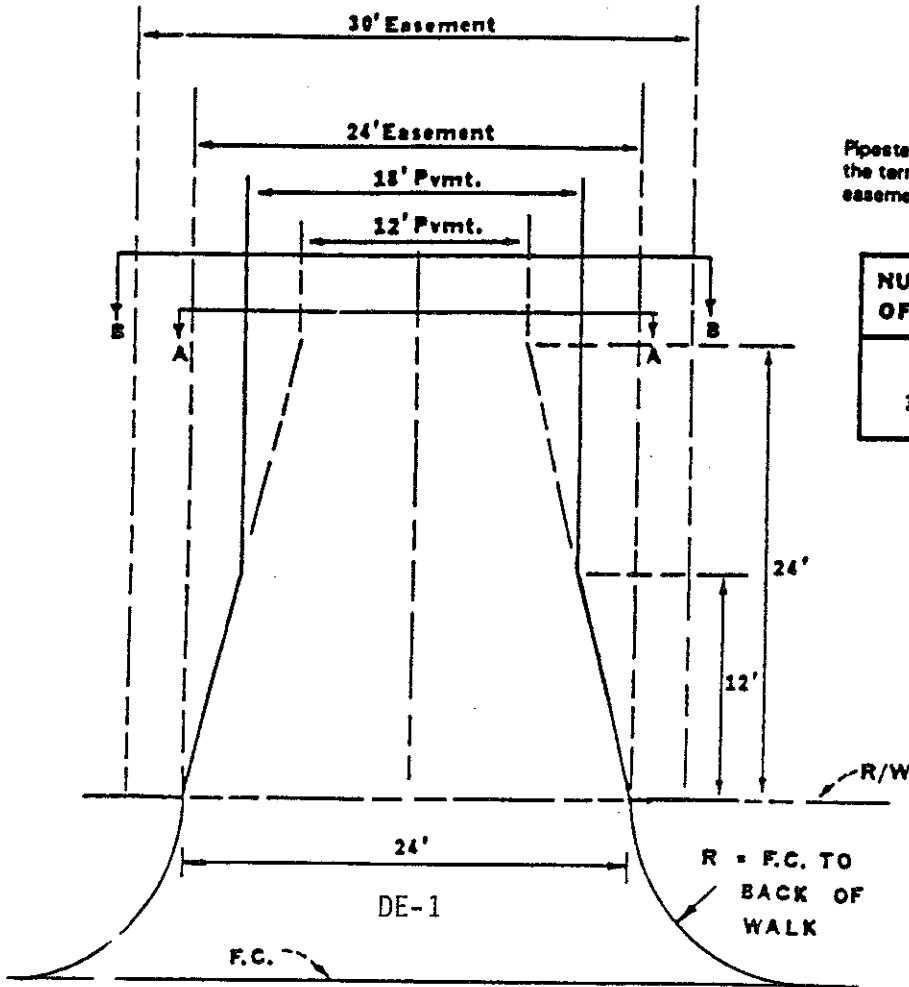
Grading Plans must provide for adequate vehicular clearance for driveway approach, departure and breakover transitions. Driveway profiles are required where steep grades prevail.

Driveways Shall Be Graded To Insure Positive Drainage Away From House.

Driveway Clearances

PLATE IV-F

Grading Plans must provide for adequate vehicle clearance for driveway approach, departure and breakover transitions. Driveway profiles are required where steep grades prevail.

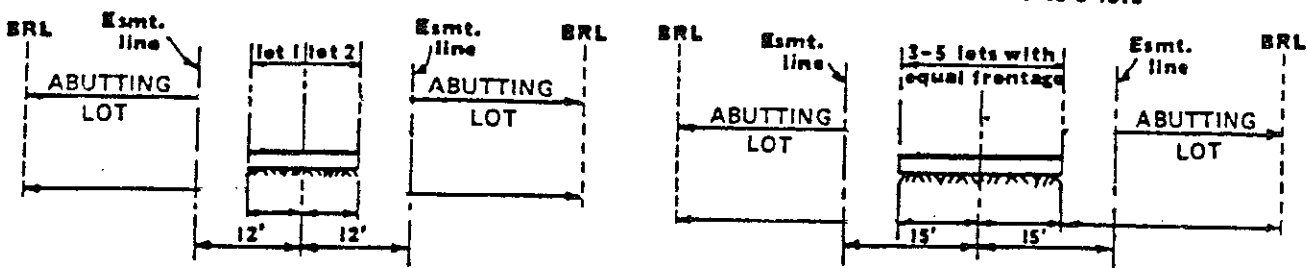


Pipestem driveways shall be paved to the terminal point of the driveway easement

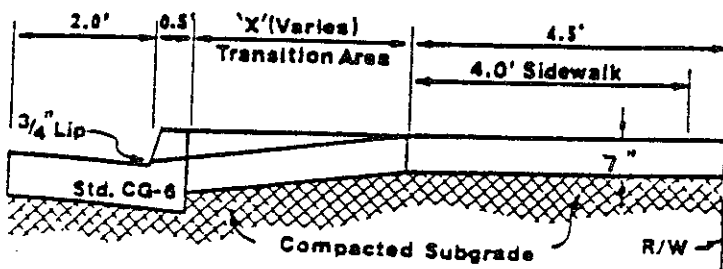
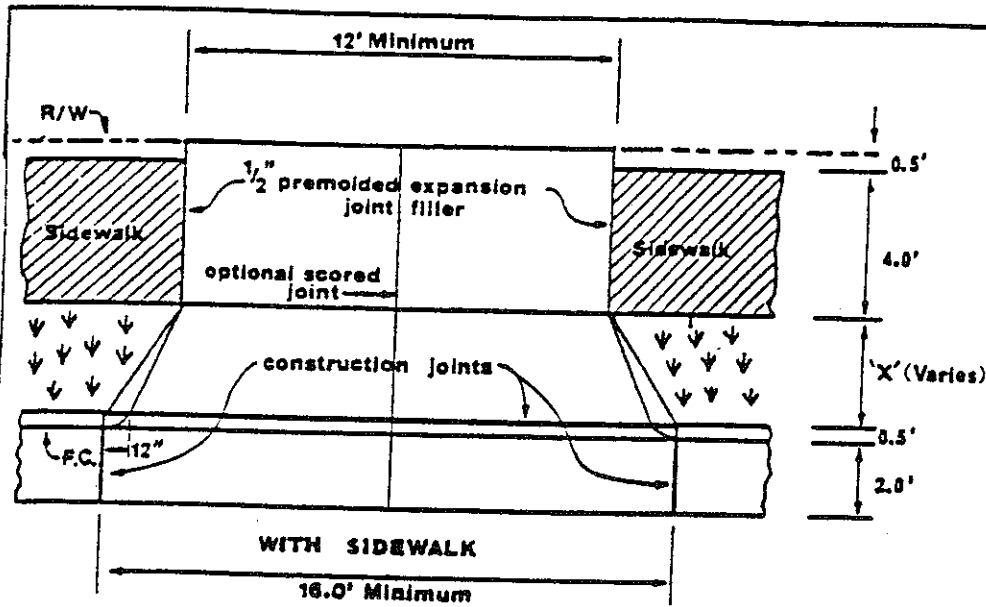
NUMBER OF LOTS	WIDTH OF EASEMENT	WIDTH OF PAVEMENT
2	24'	12'
3-5	30'	18'

SECTION A-A: 2 lots

SECTION B-B: 3 to 5 lots



STANDARD ENTRANCE
PIPE STEM LOTS
CURB & GUTTER SECTIONS



Driveway Clearances-

Lot grading plans must provide for adequate vehicular clearance for driveway approach, departure and breakover transitions. Driveway profiles are required where steep grades prevail.

All materials and construction of this design in a right of way to be maintained by COMP shall conform to the current VDOT Road and Bridge Specifications and VDOT Road and Bridge Standards.

**STANDARD DRIVEWAY
ENTRANCE**

REVISION NO.	DATE	STD. NO.
		DE-1
		PAGE NO

